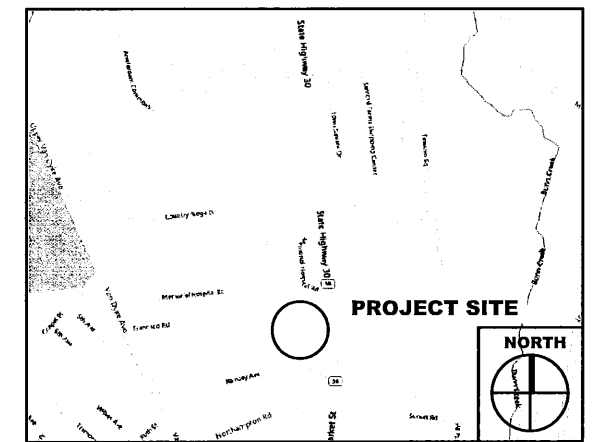
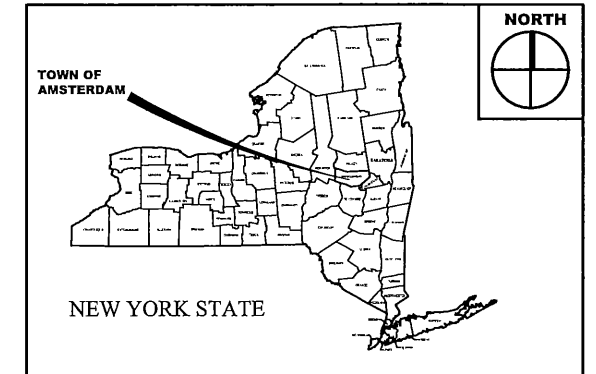
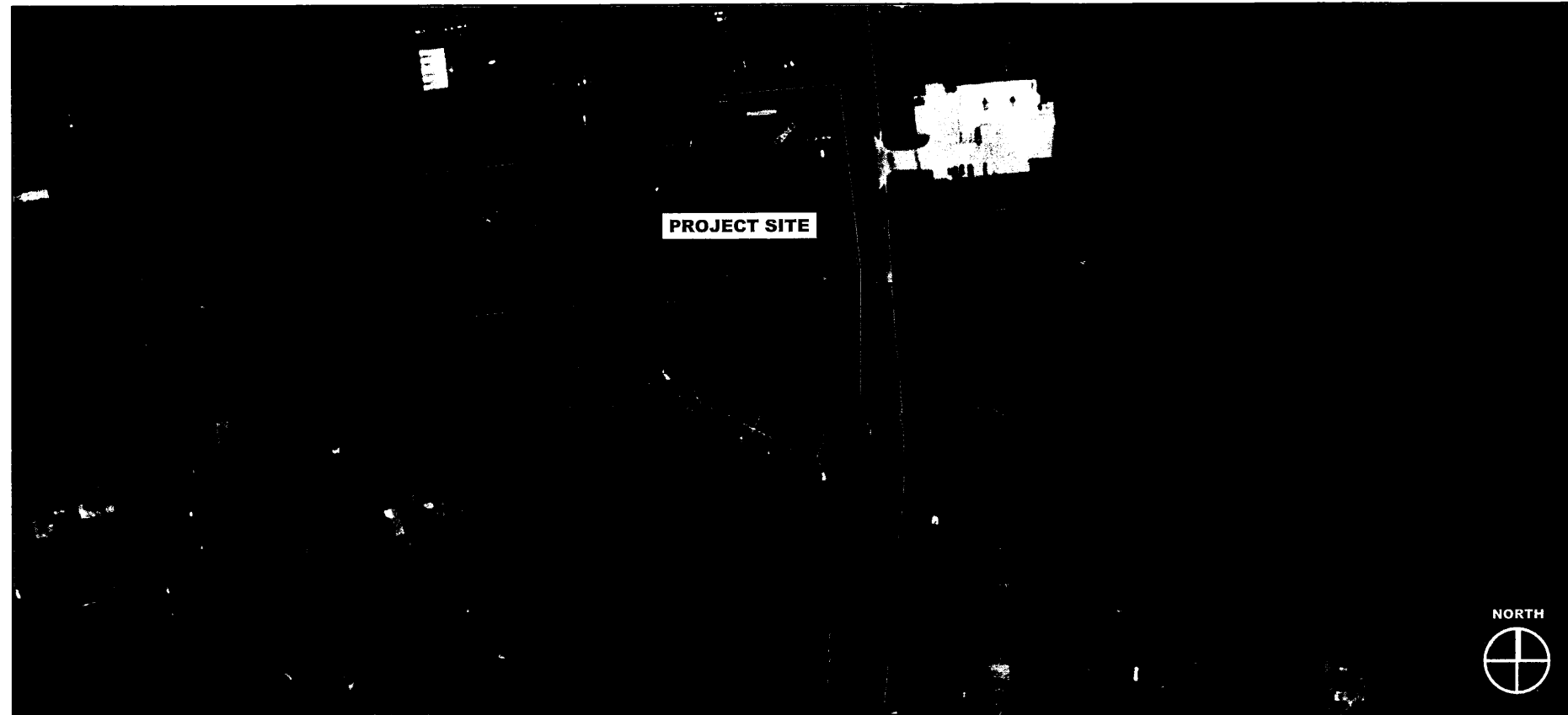


HILLCREST SPRING ASSISTED LIVING FACILITY SITE PLAN

5044 & 5052 NEW YORK STATE ROUTE 20
JUNE, 2022



SITE LOCATION MAP

DRAWING LIST

SHEET ID	SHEET TITLE	SHEET NUMBER
G-001	COVER SHEET	01
C-101	EXISTING CONDITIONS	02
C-102	OVERALL SITE PLAN	03
C-103	LANDSCAPING PLAN	04
C-104	ENTRANCE PLAN	05
C-501	SITE DETAILS AND NOTES	06
C-701	TRAFFIC AND MAINTENANCE CONTROL DETAILS	07
C-702	TRAFFIC AND MAINTENANCE CONTROL DETAILS	08
C-703	TRAFFIC AND MAINTENANCE CONTROL DETAILS	09

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WARNING: IT IS A VIOLATION OF THIS LAW FOR ANY PERSON, UNLESS HE IS ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE SEAL OF AN ENGINEER OR LAND SURVEYOR IS ALTERED, THE ALTERING ENGINEER OR LAND SURVEYOR SHALL AFFIX TO THE ITEM HIS SEAL AND THE NOTATION "ALTERED BY" FOLLOWED BY HIS SIGNATURE AND THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. PROFESSIONAL ENGINEERING AND LAND SURVEYING - ART. 145, SECTION: 7209

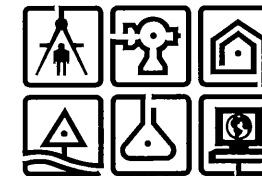
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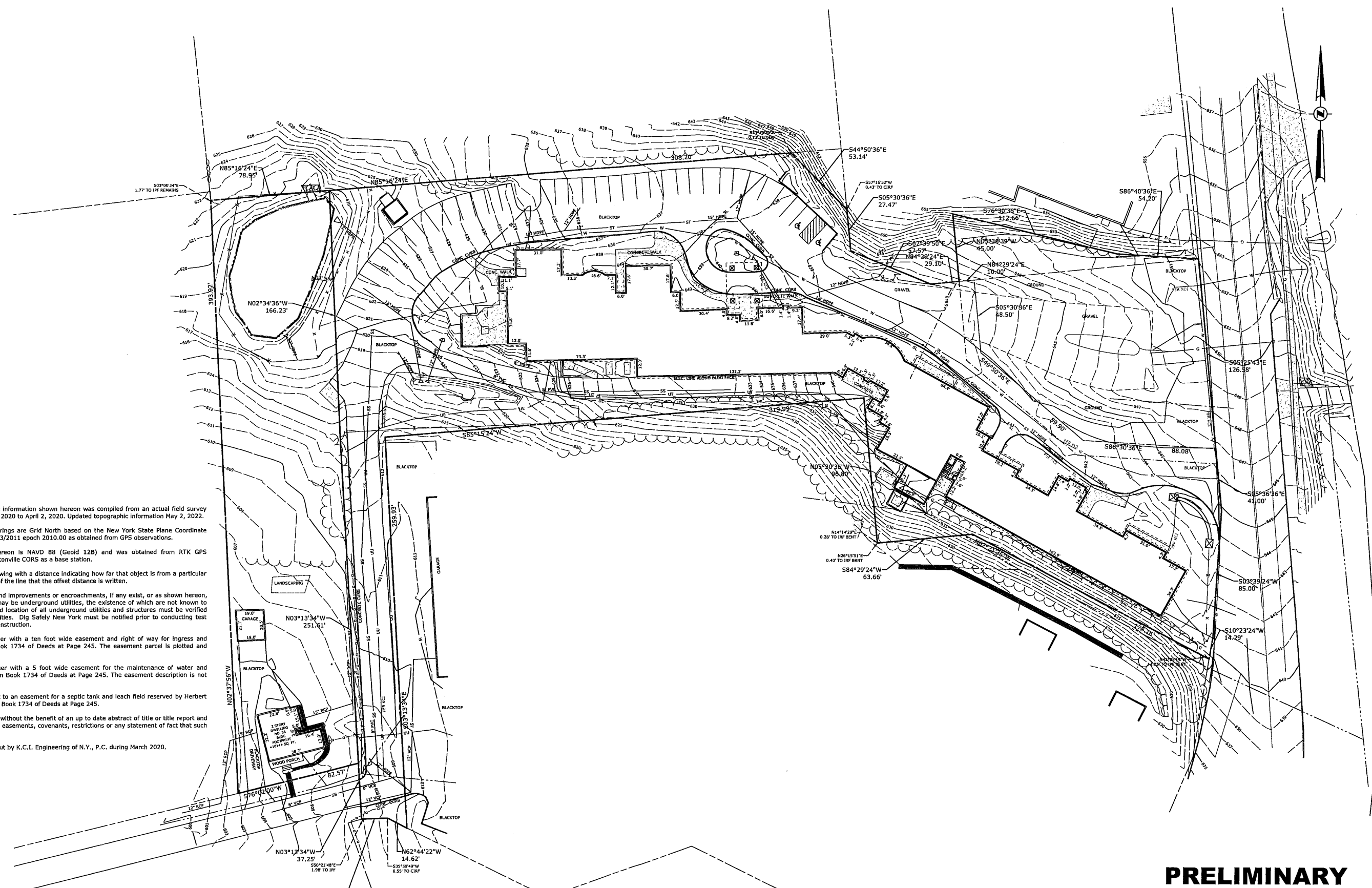


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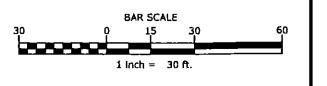
G-001

SHEET 01 OF 13



- MAP NOTES:**
1. Boundary and topographic information shown hereon was compiled from an actual field survey conducted from March 30, 2020 to April 2, 2020. Updated topographic information May 2, 2022.
 2. North orientation and bearings are Grid North based on the New York State Plane Coordinate System, East Zone, NAD 83/2011 epoch 2010.00 as obtained from GPS observations.
 3. Vertical datum shown hereon is NAVD 88 (Geoid 12B) and was obtained from RTK GPS observations using the Fultonville CORS as a base station.
 4. Objects shown on this drawing with a distance indicating how far that object is from a particular line, lie on the same side of the line that the offset distance is written.
 5. The location of underground improvements or encroachments, if any exist, or as shown hereon, are not certified. There may be underground utilities, the existence of which are not known to the undersigned. Size and location of all underground utilities and structures must be verified by the appropriate authorities. Dig Safely New York must be notified prior to conducting test borings, excavation and construction.
 6. Surveyed parcel is together with a ten foot wide easement and right of way for ingress and egress as described in Book 1734 of Deeds at Page 245. The easement parcel is plotted and shown hereon.
 7. Surveyed parcel is together with a 5 foot wide easement for the maintenance of water and sewer lines as described in Book 1734 of Deeds at Page 245. The easement description is not plottable.
 8. Surveyed parcel is subject to an easement for a septic tank and leach field reserved by Herbert Van Voast, Jr. as stated in Book 1734 of Deeds at Page 245.
 9. This survey was prepared without the benefit of an up to date abstract of title or title report and is therefore subject to any easements, covenants, restrictions or any statement of fact that such documents may disclose.
 10. Underground utility markout by K.C.I. Engineering of N.Y., P.C. during March 2020.

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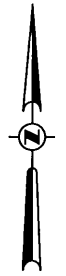
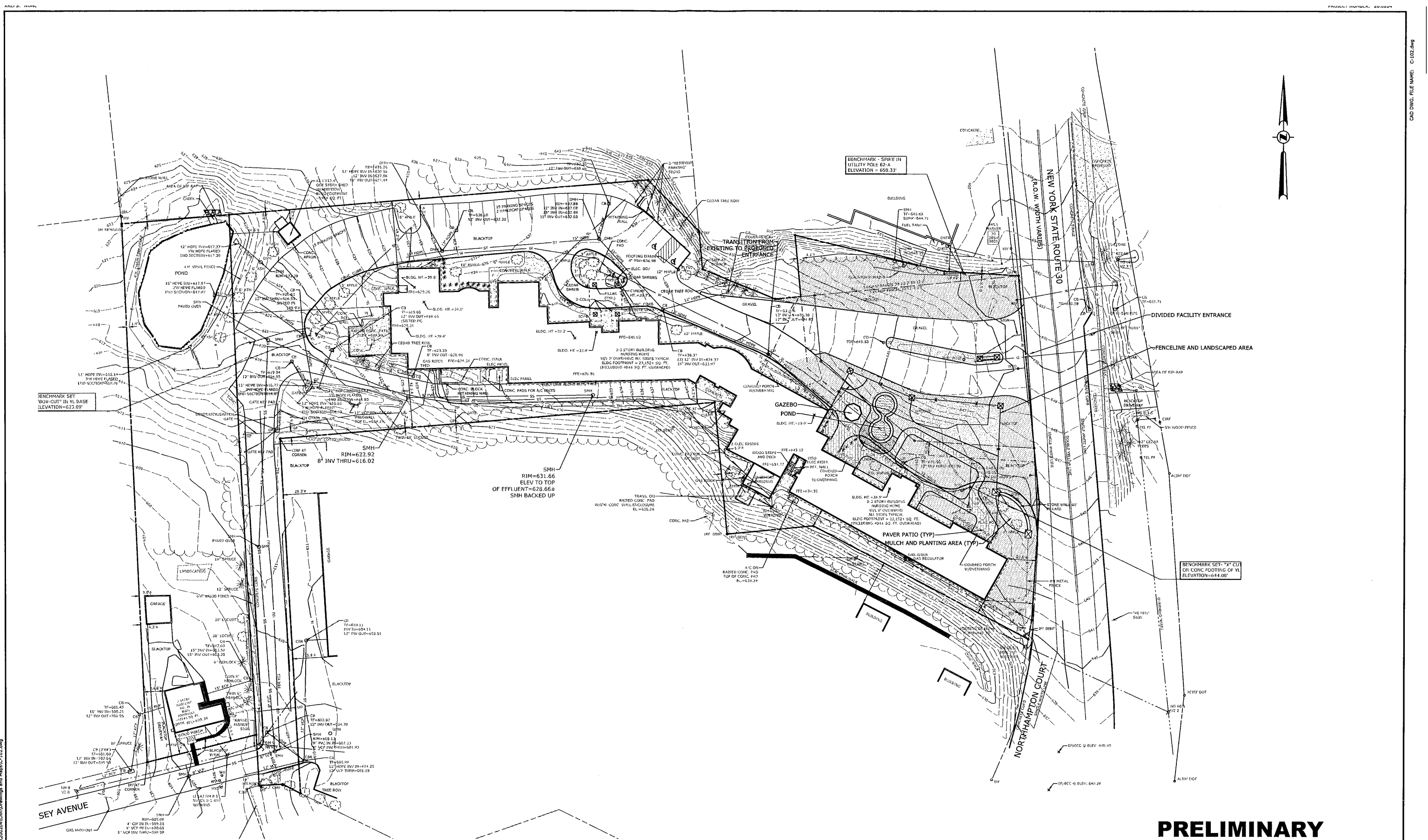


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							C-101
							SHEET 2 OF 9 DWG. NO: 22-0469

CAD DWG. FILE NAME: C-101.dwg



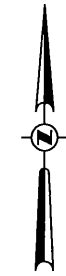
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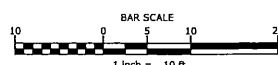
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© 2022 C.T. MALE ASSOCIATES DESIGNED: MLS DRAFTED: MLS CHECKED: CRK PROJ. NO: 20.0204 SCALE: 1"=30' DATE: JUNE 20, 2022		OVERALL SITE PLAN HILLCREST SPRING ASSISTED LIVING FACILITY 5044 & 5052 NEW YORK STATE ROUTE 30 TOWN OF AMSTERDAM MONTGOMERY COUNTY, NEW YORK	
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BLDG. HT. = 25.5'
 2-3 STORY BUILDING
 TRUSSING HOME
 W/ 4' OVERHANG
 ALL SIDES TYPICAL
 FLOOR FOOTPRINT = 72,152.50 FT.
 (INCLUDING 4044 SQ. FT. OVERHEAD)



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LANDSCAPING PLAN

HILLCREST SPRING ASSISTED LIVING FACILITY
 5044 & 5052 NEW YORK STATE ROUTE 30

TOWN OF AMSTERDAM MONTGOMERY COUNTY, NEW YORK

C.T. MALE ASSOCIATES
 Engineering, Surveying, Architecture, Landscape Architecture & Geology, D.P.C.
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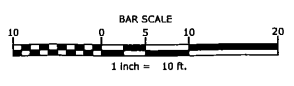
C-103
 SHEET 4 OF 9
 DWG. NO: 22-0469



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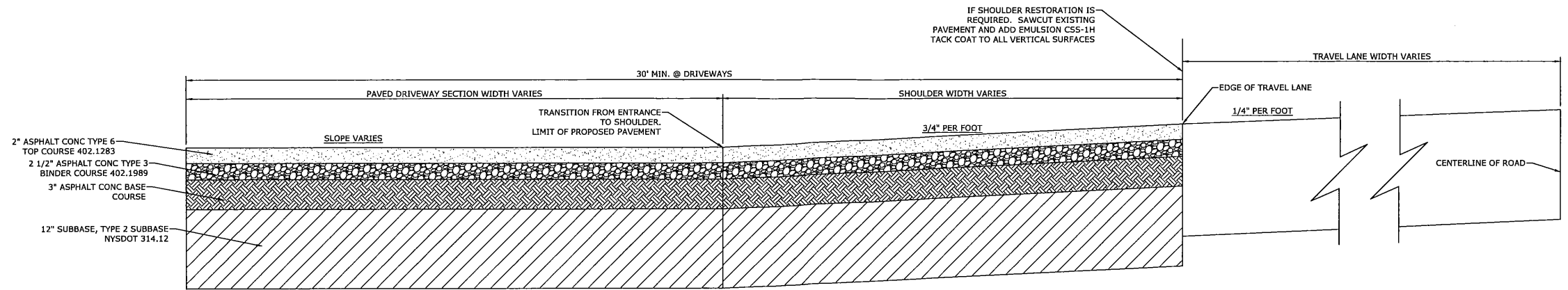
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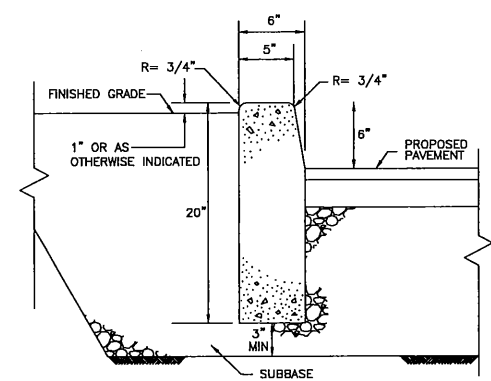
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						DATE: JUNE 20, 2022

ENTRANCE PLAN	
HILLCREST SPRING ASSISTED LIVING FACILITY 5044 & 5052 NEW YORK STATE ROUTE 30	
TOWN OF AMSTERDAM	MONTGOMERY COUNTY, NEW YORK
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C-104	SHEET 5 OF 9 DWG. NO: 22-0469

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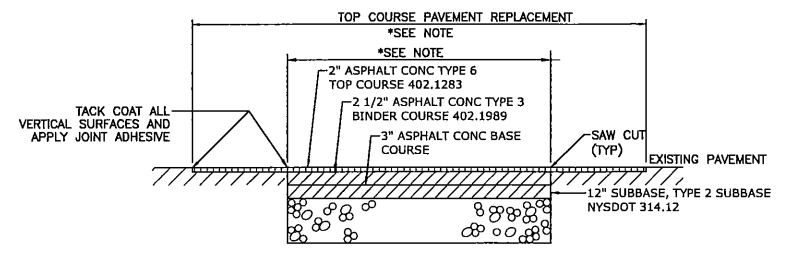


1 SHOULDER RECONSTRUCTION
 SCALE: NONE
 CROSS REFERENCE: NONE



NOTE:
 CONCRETE SHALL HAVE A 28-DAY COMPRESSIVE STRENGTH (f'_c) OF 4,000 PSI AND HAVE BETWEEN 5% AND 8% ENTRAINED AIR.

2 CONCRETE CURB DETAIL
 SCALE: NONE
 CROSS REFERENCE: NONE



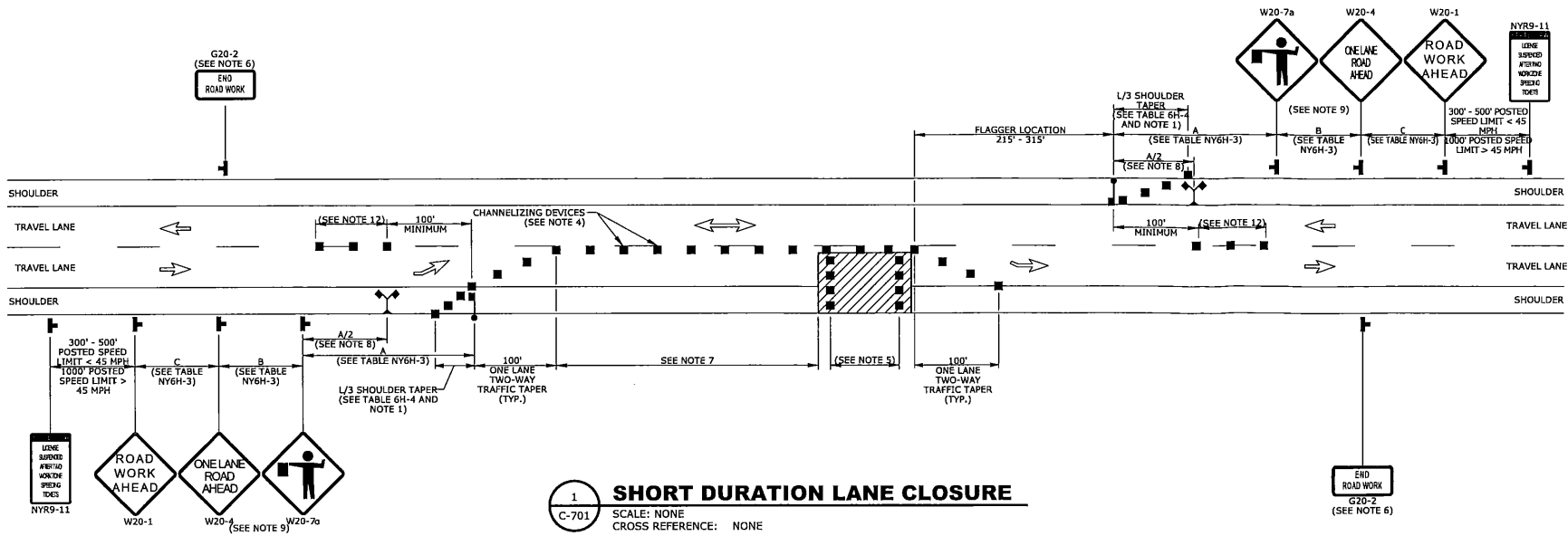
NOTE:
 1. PAYMENT LIMIT AS NOTED ON PLAN SHEETS.

3 ENTRANCE PAVEMENT DETAIL
 SCALE: NONE
 CROSS REFERENCE: NONE

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							DRAFTED: MLS	C.T. MALE ASSOCIATES	
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							SCALE: AS NOTED	COBLESKILL, NY • GLENS FALLS, NY • POUGHKEEPSIE, NY	
							DATE: JUNE 20, 2022	JOHNSTOWN, NY • RED HOOK, NY • SYRACUSE, NY	

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1 SHORT DURATION LANE CLOSURE
 SCALE: NONE
 CROSS REFERENCE: NONE

LANE CLOSURE NOTES:

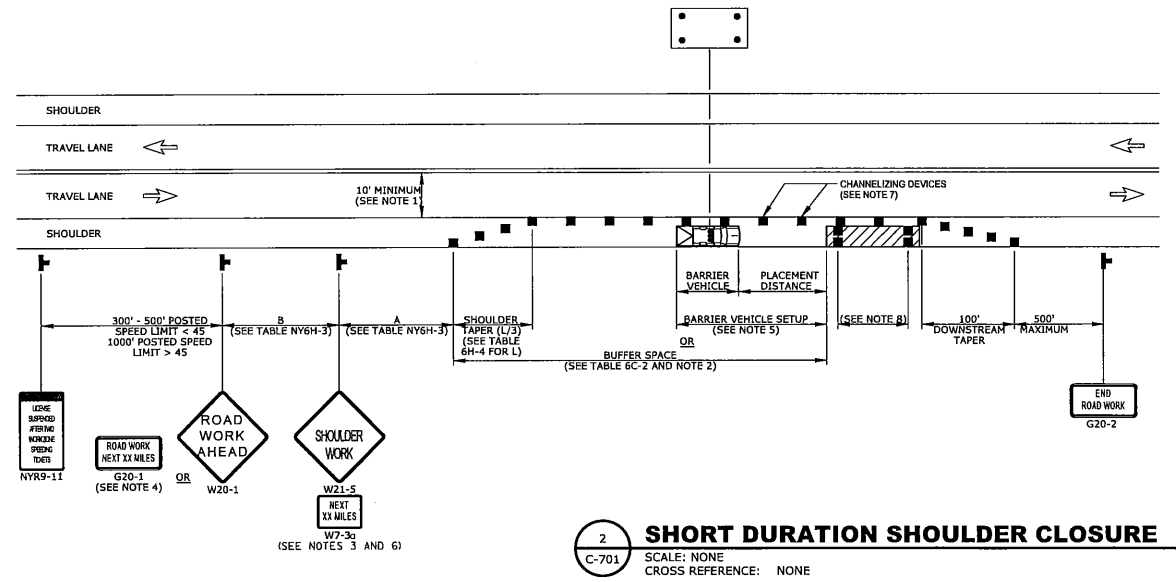
- WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK AREA AND DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVEL WAY.
- WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE ROADWAY WITHIN A WORK ZONE TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES AND/OR FLAGGERS SHALL BE PLACED AS NEEDED. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK SPACE.
- NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
- CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 40' IN THE ACTIVE WORK SPACE.
- TRANSVERSE DEVICES SHALL BE REQUIRED (AS PER 619 STANDARD SPECIFICATIONS) WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 1500'.
- THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 500' PAST THE END OF THE WORK SPACE.
- WHERE DIRECTED BY THE ENGINEER, A BUFFER SPACE SHALL BE PROVIDED IN ORDER TO LOCATE THE ONE-LANE, TWO-WAY TRAFFIC TAPER PRIOR TO ANY HORIZONTAL OR VERTICAL CURVE, IN ORDER TO PROVIDE ADEQUATE SIGHT DISTANCE FOR THE FLAGGERS AND/OR A QUEUE OF STOPPED VEHICLES.
- THE FLAG TREE SHALL BE LOCATED ON THE SHOULDER, AT APPROXIMATELY 1/2 THE DISTANCE BETWEEN THE FLAGGER SIGN (W20-7a) AND THE FLAGGER.
- FLAGGER SIGN (W20-7a) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED, COVERED OR TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
- FLAGGER AND FLAG TREE SHALL BE ILLUMINATED TO LEVEL II ILLUMINATION DURING NIGHT TIME OPERATIONS.
- ALL FLAGGERS SHALL USE 24" (MIN.) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6" STAFF.
- CENTERLINE CHANNELIZING DEVICES ARE OPTIONAL AND MAY BE ELIMINATED WHERE SPACE CONSTRAINTS EXIST.

GENERAL TRAFFIC MAINTENANCE NOTES:

- THE MAINTENANCE AND PROTECTION PLANS SHOWN ARE STANDARDS FOR THE MOST COMMON SITUATIONS. ADDITIONAL PROTECTION SHALL BE PROVIDED WHERE SPECIAL SITUATIONS OCCUR.
- THE CONTRACTOR MAY SUBMIT REVISIONS TO THIS PLAN FOR APPROVAL. BUT ANY CHANGE THAT ALTERS THE BASIC CONCEPTS OF THE PLAN MUST BE APPROVED BY THE COUNTY COMMISSIONER OF PUBLIC WORKS, OR HIS DESIGNEE.
- NO TEMPORARY LANE CLOSURES SHALL BE ALLOWED BEFORE SUNRISE OR AFTER SUNSET, OR AT OTHER TIMES WHEN VISIBILITY IS REDUCED TO LESS THAN 1000 FEET, UNLESS DIRECTED BY THE ENGINEER.
- TRAVEL LANE WIDTH SHALL BE 10 FT MINIMUM AT ALL TIMES.
- VEHICLES BELONGING TO THE CONTRACTOR, OR THE CONTRACTOR'S EMPLOYEES, SHALL NOT BE PARKED ON THE PAVEMENT OR SHOULDER, OR WITHIN 20 FEET OF THE EDGE OF PAVEMENT ALONG OR ADJACENT TO TRAVEL LANES OPEN TO TRAVEL, WITHIN THE PROJECT LIMITS.
- THE CONTRACTOR SHALL NOT PARK EQUIPMENT OR STORE MATERIAL OVERNIGHT WHERE IT IS DEEMED BY THE ENGINEER TO BE A HAZARD TO TRAFFIC.
- ALL VEHICLES THAT WILL BE MOVING IN OR OUT OF TRAFFIC AT WORK AREAS SHALL BE EQUIPPED WITH AN APPROVED AMBER ROTATING SAFETY LIGHT. THE LIGHTS SHALL BE MOUNTED SO AS TO BE EASILY SEEN BY APPROACHING TRAFFIC.
- DRIVING AGAINST TRAFFIC AT ANY TIME, REGARDLESS OF WHETHER THE AREA HAS BEEN CLOSED TO TRAFFIC, SHALL NOT BE PERMITTED, EXCEPT FOR TRAFFIC CONE PICK-UP, AND AS SPECIFICALLY PERMITTED BY THE ENGINEER.
- PRIOR TO ANY REDUCTION IN ROADWAY WIDTH, THE CONTRACTOR SHALL PROVIDE THE ENGINEER TWENTY ONE (21) DAYS NOTICE SO HE/SHE MAY CONTACT THE REGIONAL PERMIT ENGINEER OF THE WIDTH RESTRICTIONS IN A TIMELY MANNER. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IN WRITING IMMEDIATELY UPON THE REMOVAL OF THE LANE WIDTH RESTRICTION SO THE ENGINEER MAY NOTIFY THE REGIONAL PERMIT ENGINEER.
- EXCAVATIONS THAT PRODUCE DROP-OFFS ON BOTH SIDES OF THE TRAVEL WAY AT THE SAME TIME SHALL NOT BE PERMITTED, SHOULDERS SHOULD BE PREPARED TO RECEIVE THE SHOULDER PAVEMENT MATERIAL IMMEDIATELY AHEAD OF THE SHOULDER PAVING OPERATIONS TO MINIMIZE THE TIME A DROP-OFF EXISTS. "NO SHOULDER" (W4-13C) SIGNS SHALL BE ERRECTED NO MORE THAN 1200 FEET APART THROUGHOUT THE PROJECT WHERE A DROP-OFF EXISTS.
- THE BOTTOM OF ALL TEMPORARY CONSTRUCTION SIGNS SHALL BE A MINIMUM OF 7 FEET ABOVE THE SURFACE.
- CONTRACTOR SHALL PROVIDE AND MAINTAIN SAFE AND ADEQUATE INGRESS AND EGRESS TO AND FROM HOMES AND COMMERCIAL ESTABLISHMENTS AT ALL TIMES. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR PROVIDING SAFE PEDESTRIAN ACCESS AT ALL TIMES.
- IF THE ENGINEER NOTIFIES THE CONTRACTOR OR HIS SUPERINTENDENT OF ANY HAZARDOUS CONSTRUCTION PRACTICES, ALL OPERATIONS IN THAT AREA SHALL BE DISCONTINUED AND IMMEDIATE REMEDIAL ACTION SHALL BE TAKEN TO THE SATISFACTION OF THE ENGINEER BEFORE WORK IS RESUMED.
- IN ORDER TO MAINTAIN EFFECTIVE TRAFFIC CONTROL, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF THE TRAFFIC CONTROL. MAKING SURE ALL SIGNS, CONES, FLASHERS, DRUMS, ETC. ARE IN PLACE AND IN GOOD CONDITION. THE SOLE JUDGE OF THE EFFECTIVENESS OF THE CONTRACTOR'S EFFORTS TOWARDS THE PROTECTION OF TRAFFIC AND PERSONNEL SHALL BE THE ENGINEER'S.
- FLAGGERS SHALL BE LOCATED AT ALL ACTIVE WORK AREAS AND AT OTHER LOCATIONS WITHIN A WORK AREA WHERE RESTRICTED SIGHT DISTANCE IMPEDES THE FLOW OF TRAFFIC.
- EXISTING TRAFFIC SIGNS SHALL BE COVERED AND UNCOVERED AS NECESSARY DURING CONSTRUCTION.
- IN THE ENGINEER'S JUDGEMENT, FLAGS ON SIGNS ARE NECESSARY DUE TO LIMITED SIGHT DISTANCE THEY SHALL BE PROVIDED BY THE CONTRACTOR.
- THE WORK AREA IS TO BE CLEANED DAILY AND LEFT IN AN ACCEPTABLE MANNER.
- PEDESTRIAN AND BICYCLIST TRAFFIC SHALL BE MAINTAINED AND PROTECTED AT ALL TIMES. CONTRACTOR SHALL KEEP ONE DRIVING LANE AND ONE SIDEWALK OPEN AT ALL TIMES.

SHOULDER CLOSURE NOTES:

- WHEN THE MINIMUM LANE WIDTH OF 10' CANNOT BE MAINTAINED DUE TO A SHOULDER CLOSURE, USE THE DETAIL FOR SHORT OR INTERMEDIATE TERM STATIONARY FLAGGING OPERATION.
- NO WORK ACTIVITY OR STORAGE OF EQUIPMENT, VEHICLES, OR MATERIAL SHOULD OCCUR WITHIN A BUFFER SPACE.
- WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK IS 2 MILES TO 5 MILES, A SUPPLEMENTAL DISTANCE PLAQUE (W7-3a) SHOULD BE USED WITH THE "SHOULDER WORK SIGN (W21-5).
- THE ROAD WORK NEXT XX MILES SIGN (G20-1) MAY BE USED INSTEAD OF THE ROAD WORK AHEAD SIGN (W20-1) IF WORK LOCATIONS OCCUR OVER A DISTANCE OF MORE THAN 2 MILES.
- FOR BARRIER VEHICLE USE REQUIREMENTS SEE TABLES NY1-A AND NY2-A ON THE STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL LEGENDS AND NOTES".
- IN SITUATIONS WHERE MULTIPLE WORK LOCATIONS EXIST WITHIN A LIMITED DISTANCE, THE DISTANCE BETWEEN THE ADVANCE WARNING SIGN AND WORK SHALL NOT EXCEED 5 MILES.
- CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 40' IN THE ACTIVE WORK SPACE.
- TRANSVERSE DEVICES SHALL BE REQUIRED (AS PER 619 STANDARD SPECIFICATIONS) WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 1500'.



2 SHORT DURATION SHOULDER CLOSURE
 SCALE: NONE
 CROSS REFERENCE: NONE

PRELIMINARY

TRAFFIC AND MAINTENANCE CONTROLS

HILLCREST SPRING ASSISTED LIVING FACILITY
5044 & 5052 NEW YORK STATE ROUTE 30

TOWN OF AMSTERDAM MONTGOMERY COUNTY, NEW YORK

C.T. MALE ASSOCIATES
 Engineering, Surveying, Architecture, Landscape Architecture & Geology, D.P.C.

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C-701
 SHEET 7 OF 9
 DWG. NO: 22-0469

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					SCALE: AS NOTED
					DATE: JUNE 20, 2022

CHARLES R. KORTZ
 P.E. NO. 081516

TABLE NY1-A BARRIER VEHICLE USE REQUIREMENTS (LONG TERM, INTERMEDIATE TERM, AND SHORT TERM STATIONARY CLOSURES)					
CLOSURE TYPE	EXPOSURE CONDITION	FREEWAY	USE REQUIREMENTS 4,5		
			NON-FREEWAY (PRECONSTRUCTION POSTED SPEED LIMIT)		
			≥ 45 MPH	35-40 MPH	≤ 30 MPH
LANE CLOSURE	WORKERS ON FOOT OR IN VEHICLES EXPOSED TO TRAFFIC	REQUIRED 3	REQUIRED 3	REQUIRED 3	OPTIONAL 2
	NON-TRAVERSABLE HAZARD (IE. EQUIPMENT, MATERIALS, EXCAVATION) ONLY NO WORKERS EXPOSED	REQUIRED 3	REQUIRED 3	OPTIONAL 2	OPTIONAL 2
SHOULDER CLOSURE	WORKERS ON FOOT OR IN VEHICLES EXPOSED TO TRAFFIC	REQUIRED 3	REQUIRED 3	OPTIONAL 2	OPTIONAL 2
	NON-TRAVERSABLE HAZARD (IE. EQUIPMENT, MATERIALS, EXCAVATION) ONLY NO WORKERS EXPOSED	REQUIRED 3	OPTIONAL 2	OPTIONAL 2	OPTIONAL 2

- THE EXPOSURE CONDITIONS DESCRIBED IN TABLE NY1-A ASSUMES THERE IS NO POSITIVE PROTECTION (TEMPORARY TRAFFIC BARRIER) PRESENT. WHERE WORKERS OR HAZARDS ARE PROTECTED BY A TEMPORARY TRAFFIC BARRIER, BARRIER VEHICLES ARE NOT REQUIRED.
- WHERE THE REQUIREMENT IS "OPTIONAL," EITHER A BARRIER VEHICLE OR THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.
- REQUIREMENTS SHALL INCLUDE PROVIDING A SEPARATE BARRIER VEHICLE FOR EACH CLOSED LANE AND EACH CLOSED PAVED SHOULDER 8' OR GREATER IN WIDTH. IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE BARRIER VEHICLE SHALL BE REPOSITIONED ACCORDINGLY. BARRIER VEHICLES PROTECTING NON-TRAVERSABLE HAZARDS SHALL REMAIN IN PLACE DURING BOTH WORKING AND NON-WORKING HOURS UNTIL THE HAZARD NO LONGER EXISTS. EXCEPTIONS TO THESE REQUIREMENTS MAY BE MADE, AS APPROVED BY THE COUNTY COMMISSIONER OF PUBLIC WORKS OR HIS/HER DESIGNEE WHERE BARRIER VEHICLE PLACEMENT WOULD BE INEFFECTIVE OR WOULD INTERFERE WITH THE SAFE OPERATION OF TRAFFIC.
- BARRIER VEHICLES ARE NOT REQUIRED FOR MILLING AND/OR PAVING OPERATIONS, BUT THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.
- BARRIER VEHICLES ARE NOT REQUIRED FOR FLAGGING OPERATIONS, BUT THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.

TABLE NY1-B SHADOW VEHICLE USE REQUIREMENTS (MOBILE CLOSURES)					
CLOSURE TYPE	EXPOSURE CONDITION	FREEWAY	USE REQUIREMENTS		
			NON-FREEWAY (PRECONSTRUCTION POSTED SPEED LIMIT)		
			≥ 45 MPH	35-40 MPH	≤ 30 MPH
LANE CLOSURE	WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC	REQUIRED 2,4	REQUIRED 2,4	REQUIRED 2,4	REQUIRED 2,4
SHOULDER CLOSURE	WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC	REQUIRED 2,4	REQUIRED 2,4	REQUIRED 2,4	REQUIRED 2,4

- A MOBILE CLOSURE SHALL BE USED FOR ANY WORK ACTIVITY THAT MOVES CONTINUOUSLY OR INTERMITTENTLY ALONG THE TRAVELED WAY OR SHOULDER SLOWER THAN THE PREVAILING SPEED OF TRAFFIC. CHANNELIZING DEVICES ARE NOT USED FOR MOBILE CLOSURES.
- SHADOW VEHICLES SHALL BE EQUIPPED WITH AN APPROVED REAR MOUNTED ATTENUATOR (TRUCK MOUNTED OR TRAILER MOUNTED) FOR THE FOLLOWING MOBILE CLOSURES: LANE CLOSURES ON FREEWAYS, LANE CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION POSTED SPEED LIMIT OF 35 MPH OR MORE, SHOULDER CLOSURES ON FREEWAYS, AND SHOULDER CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION SPEED LIMIT OF 45 MPH OR MORE.
- FOR MOBILE LANE CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION POSTED SPEED LIMIT OF 30 MPH OR LESS AND MOBILE SHOULDER CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION SPEED LIMIT OF 40 MPH OR LESS, SHADOW VEHICLES ARE NOT REQUIRED TO BE EQUIPPED WITH A REAR MOUNTED ATTENUATOR.
- A SHADOW VEHICLE IS USED TO PROTECT EXPOSED WORKERS (ON FOOT OR IN A VEHICLE) AND SHALL BE REQUIRED FOR ALL MOBILE CLOSURES. SHADOW VEHICLE REQUIREMENTS SHALL INCLUDE PROVIDING A SEPARATE SHADOW VEHICLE FOR EACH CLOSED LANE AND EACH CLOSED PAVED SHOULDER 8' OR GREATER IN WIDTH. ADDITIONAL SHADOW VEHICLES MAY BE REQUIRED TO PROMOTE THE SAFE OPERATION OF TRAFFIC AND THE INCREASED PROTECTION OF EXPOSED WORKERS, AS DIRECTED BY THE COUNTY COMMISSIONER OF PUBLIC WORKS OR HIS/HER DESIGNEE.

TEMPORARY LANE/SHOULDER CLOSURE RESTRICTIONS FOR HOLIDAYS 2022		
HOLIDAY	FALLS ON	TEMPORARY LAND CLOSURES ARE NOT ALLOWED FROM
NEW YEAR'S DAY	SATURDAY JANUARY 1	BEGINNING 6 AM THURSDAY DECEMBER 30, 2021, AND ENDING 6 AM MONDAY JANUARY 3, 2022
MEMORIAL DAY	MONDAY MAY 30	BEGINNING 6 AM FRIDAY MAY 27, 2022, AND ENDING 6 AM TUESDAY MAY 31, 2022
INDEPENDENCE DAY	MONDAY JULY 4	BEGINNING 6 AM FRIDAY JULY 1, 2022, AND ENDING 6 AM TUESDAY JULY 5, 2022
LABOR DAY	MONDAY SEPTEMBER 5	BEGINNING 6 AM FRIDAY SEPTEMBER 2, 2022, AND ENDING 6 AM TUESDAY SEPTEMBER 6, 2022
THANKSGIVING DAY	THURSDAY NOVEMBER 24	BEGINNING 6 AM WEDNESDAY NOVEMBER 24, 2022, AND ENDING 6 AM MONDAY NOVEMBER 28, 2022
CHRISTMAS DAY	SUNDAY DECEMBER 25	BEGINNING 6 AM FRIDAY DECEMBER 23, 2022, AND ENDING 6 AM TUESDAY DECEMBER 27, 2022

TABLE 6H-4 FORMULAS FOR DETERMINING TAPER LENGTHS										
SPEED LIMIT (S) (MPH)	TAPER LENGTH (L) (FT.)		L = TAPER LENGTH W = WIDTH OF OFFSET (FT.) S = PRECONSTRUCTION POSTED SPEED LIMIT (MPH)							
(40 MPH) OR LESS	L = WS / 60									
(45 MPH) OR MORE	L = WS									
STANDARD TAPER LENGTHS										
LATERAL SHIFT OF TRAFFIC FLOW PATH	TEMPORARY TRAFFIC CONTROL ZONE POSTED SPEED LIMIT									
	(25 MPH)	(30 MPH)	(35 MPH)	(40 MPH)	(45 MPH)	(50 MPH)	(55 MPH)	(60 MPH)	(65 MPH)	(70 MPH)
4	45	60	85	110	180	200	220	240	260	280
5	55	75	105	135	225	250	275	300	325	350
6	65	90	125	160	270	300	330	360	390	420
7	75	105	145	190	315	350	385	420	455	490
8	85	120	165	215	360	400	440	480	520	560
9	95	135	185	240	405	450	495	540	585	630
10	105	150	205	270	450	500	550	600	650	700
11	115	165	225	295	495	550	605	660	715	770
12	125	180	245	320	540	600	660	720	780	840

TABLE 6C-2 LONGITUDINAL BUFFER SPACE	
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	DISTANCE
25	155 FT.
30	200 FT.
35	250 FT.
40	305 FT.
45	360 FT.
50	425 FT.
55	495 FT.
60	570 FT.
65	645 FT.

TABLE NY2-A PLACEMENT DISTANCE FOR BARRIER VEHICLES				
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	PLACEMENT DISTANCE (FT.)			
	BARRIER VEHICLES*			
	(18000 LBS.)		(24000 LBS.)	
	MINIMUM	MAXIMUM	MINIMUM	MAXIMUM
> 55	100 FT.	200 FT.	100 FT.	200 FT.
45 - 55	100 FT.	200 FT.	85 FT.	165 FT.
< 45	85 FT.	165 FT.	50 FT.	100 FT.

* AS DEFINED IN NYS DOT STANDARD SPECIFICATION 619:
BARRIER VEHICLE - VEHICLE USED FOR STATIONARY SHOULDER CLOSURES, LANE CLOSURES, AND OTHER STATIONARY WORK ZONES.
MINIMUM DISTANCE SHOWN REFLECTS THE ACTUAL ROLL AHEAD DISTANCE FROM MANUFACTURER.

TABLE NY2-B PLACEMENT DISTANCE FOR SHADOW VEHICLES				
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	PLACEMENT DISTANCE (FT.)			
	SHADOW VEHICLES**			
	(18000 LBS.)		(24000 LBS.)	
	MINIMUM	MAXIMUM	MINIMUM	MAXIMUM
> 55	230 FT.	330 FT.	180 FT.	280 FT.
45 - 55	180 FT.	280 FT.	150 FT.	250 FT.
< 45	100 FT.	200 FT.	100 FT.	200 FT.

* AS DEFINED IN NYS DOT STANDARD SPECIFICATION 619:
SHADOW VEHICLE - VEHICLE USED FOR MOBILE OR SHORT DURATION WORK OPERATIONS.
MINIMUM DISTANCE SHOWN REFLECTS THE ACTUAL ROLL AHEAD DISTANCE FROM MANUFACTURER.

TABLE 6C-3 TAPER LENGTH FOR TEMPORARY TRAFFIC CONTROL ZONES	
TYPE OF TAPER	TAPER LENGTH (L)
MERGING TAPER	L
SHIFTING TAPER	L/2
SHOULDER TAPER	L/3
ONE-LANE, TWO-WAY TRAFFIC TAPER	100 FT. MAXIMUM
DOWNSTREAM TAPER	100 FT. PER LANE

TABLE 619-4 FLARE RATES FOR POSITIVE BARRIER					
TYPE OF POSITIVE BARRIER	POSTED SPEED LIMIT				
	30 MPH	40 MPH	50 MPH	55 MPH	65 MPH
TEMPORARY CONCRETE BARRIER	8:1	11:1	14:1	16:1	20:1
BOX BEAM OR HEAVY POST CORRUGATED BEAM	7:1	9:1	11:1	12:1	15:1

TABLE NYH-3 ADVANCE WARNING SIGN SPACING					
ROAD TYPE	DISTANCE BETWEEN SIGNS			SIGN LEGEND	
	A (FT.)	B (FT.)	C (FT.)	XX	YY
URBAN (≤ 30 MPH*)	100	100	100	AHEAD	AHEAD
URBAN (35-40 MPH*)	200	200	200	AHEAD	AHEAD
URBAN (≥ 45 MPH*)	350	350	350	1000 FT.	AHEAD
RURAL	500	500	500	1500 FT.	1000 FT.
EXPRESSWAY / FREEWAY	1000	1500	2640	1 MILE	½ MILE

* PRECONSTRUCTION POSTED SPEED LIMIT
URBAN: (MEETS MORE THAN 1 OF THE FOLLOWING CRITERIA) SIDEWALKS, BICYCLE USAGE, CURBING, CLOSED DRAINAGE SYSTEMS, DRIVEWAY DENSITIES GREATER THAN 24 DRIVEWAYS PER MILE, MINOR COMMERCIAL DRIVEWAY DENSITIES OF 10 DRIVEWAYS PER MILE OR GREATER, MAJOR COMMERCIAL DRIVEWAYS, NUMEROUS RIGHT OF WAY CONSTRAINTS, HIGH DENSITY OF CROSS STREETS, 85TH PERCENTILE SPEEDS OF 45 MPH OR LESS.
RURAL: ANY AREA NOT EXHIBITING MORE THAN ONE OF THE ABOVE CHARACTERISTICS.
EXPRESSWAY: DIVIDED HIGHWAYS FOR TRAFFIC WITH FULL OR PARTIAL CONTROL OF ACCESS AND GENERALLY WITH GRADE SEPARATIONS AT MAJOR CROSSROADS.
FREEWAYS/INTERSTATE: LOCAL OR INTER REGIONAL HIGH-SPEED, DIVIDED, HIGH-VOLUME FACILITIES WITH FULL OR PARTIAL CONTROL OF ACCESS.

WORK DURATION DEFINITIONS
LONG-TERM STATIONARY IS WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS.
INTERMEDIATE-TERM STATIONARY IS WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.
SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR.
MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY.

WORK ZONE TRAFFIC CONTROL LEGEND	
SYMBOL	DESCRIPTION
	ARROW PANEL
	ARROW PANEL, CAUTION MODE
	ARROW PANEL TRAILER OR SUPPORT
	CHANGEABLE MESSAGE SIGN (PVMS)
	CHANNELIZING DEVICE
	CRASH CUSHION/TEMPORARY IMPACT ATTENUATOR
	DIRECTION OF TEMPORARY TRAFFIC DETOUR
	DIRECTION OF TRAFFIC
	FLAGGER
	FLAG TREE
	LUMINAIRE
	PAVEMENT MARKINGS THAT SHALL BE REMOVED FOR A LONG TERM PROJECT
	SIGN, TEMPORARY
	TEMPORARY BARRIER
	TEMPORARY BARRIER WITH WARNING LIGHTS
	TRAFFIC OR PEDESTRIAN SIGNAL
	TYPE III BARRICADE
	WARNING LIGHTS
	WORK SPACE
	WORK VEHICLE
	WORK VEHICLE WITH TRUCK MOUNTED ATTENUATOR

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WORK ZONE TRAFFIC CONTROL SIGN TABLE					
SIGN	M.U.T.C.D. CODE	COLOR CODE	CONVENTIONAL ROAD*	EXPRESSWAY	FREEWAY
	G20-1	A	36"x18"	42"x24"	42"x24"
	G20-2	A	36"x18"	42"x24"	42"x24"
	NYR9-11	B	24"x42"	48"x84"	48"x84"
	W20-7a	A	36"x36"	48"x48"	48"x48"
	W21-1a	A	36"x36"	48"x48"	48"x48"
	W21-5	A	30"x30"	48"x48"	48"x48"
	W20-1	A	36"x36"	48"x48"	48"x48"
	W20-4	A	36"x36"	48"x48"	48"x48"
	W7-3a	A	24"x18"	24"x18"	24"x18"
	W20-5	A	36"x36"	48"x48"	48"x48"
	W21-5bL	A	36"x36"	48"x48"	48"x48"
	W21-5bR	A	36"x36"	48"x48"	48"x48"

WORK ZONE TRAFFIC CONTROL SIGN TABLE					
SIGN	M.U.T.C.D. CODE	COLOR CODE	CONVENTIONAL ROAD*	EXPRESSWAY	FREEWAY
	W13-1	A	24"x24"	24"x24"	24"x24"
	W1-6L	A	48"x24"	48"x24"	48"x24"
	W1-6R	A			
	W1-8L	A	36"x48"	36"x48"	36"x48"
	W1-8R	A			

GENERAL NOTES:

- GENERAL:** ALL SIGNS, CONES, BARRELS, BARRICADES AND CONC BARRIERS SHALL BE FABRICATED AND ERECTED IN ACCORDANCE WITH THE CONTRACT PLANS AND SPECIFICATIONS OR AS DIRECTED BY THE NEW YORK STATE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES
- SIGN SUPPORT:** THE CONTRACTOR SHALL SUPPLY ADEQUATE SUPPORTS SO THAT THE SIGNS ARE IN PROPER POSITION AND ALIGNMENT AS SHOWN IN THE NEW YORK STATE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, ALL SUPPORTS SHALL BE PAINTED WITH TWO(2) COATS OF WHITE PAINT.
- SIGN PANELS:** THE SIGN PANELS MAY BE MADE OF ALUMINUM, STEEL, OR PLYWOOD THE BACKS OF ALL WOOD SIGN PANELS SHALL RECEIVE TWO(2) COATS OF WHITE PAINT.
- SIGN FACE:** **COLOR** - THE COLOR OF THE BACKGROUND AND THE LEGEND OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE NEW YORK STATE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THE MARGIN SHALL BE OF THE SAME COLOR AS SPECIFIED FOR THE BACKGROUND. THE BORDER SHALL BE OF THE SAME COLOR AS THE LEGEND. **SHAPE** - THE SHAPE OF ALL SIGNS SHALL BE AS SHOWN ON THIS SHEET. CORNERS OF SIGNS SHALL HAVE ROUNDED CORNERS. **REFLECTORIZATION** - ALL SIGNS SHALL BE REFLECTORIZED USING HIGH INTENSITY REFLECTORIZED TAPE OR PAINT. ALL SIGNS SHALL BE CLEANED AND MAINTAINED REGULARLY. ALL SIGNS SHALL BE REVIEWED AT NIGHT AFTER ERECTION. ANY SIGNS NOT MEETING PROPER REFLECTION REQUIREMENTS WILL BE REPLACED. **LETTERING AND BORDERS** - SIGN LETTERING, BORDERS AND MARGINS SHALL BE IN ACCORDANCE WITH THE NEW YORK STATE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- SIGN LOCATION:** SIGNS SHALL BE GENERALLY LOCATED ON THE RIGHT SIDE OF THE HIGHWAY FACING APPROACHING TRAFFIC. THE NEAR EDGE OF THE SIGN SHALL BE BETWEEN 6 AND 12 FEET FROM NEAREST EDGE OF THE TRAVELED ROADWAY OR BETWEEN 2 AND 12 FEET FROM THE FACE OF THE VERTICAL CURB. SIGNS SHOULD GENERALLY BE PLACED AT RIGHT ANGLES TO TRAFFIC. ROADSIDE SIGNS SHOULD BE MOUNTED SO THAT THE BOTTOM OF THE SIGN IS APPROXIMATELY 5 FEET ABOVE THE EDGE OF PAVEMENT. SIGNS MOUNTED ON BARRICADES OR TEMPORARY SIGNS IN THE ROADWAY MAY BE AT LOWER HEIGHTS. ALL SIGNS SHALL BE LOCATED SO AS TO BE PLAINLY VISIBLE TO TRAFFIC.
- MAINTENANCE:** THE CONTRACTOR SHALL KEEP SIGNS CLEANED AND CLEARED AT ALL TIMES. ALL SIGNS SHALL BE THE PROPERTY OF THE CONTRACTOR AND SHALL BE MAINTAINED IN GOOD CONDITION FOR THE DURATION OF THE CONTRACT. ALL SIGNS SHALL BE REMOVED FROM THE WORK SITE WHEN THE CONTRACT WORK IS ACCEPTED.
- GENERAL:**

COLOR CODE LEGEND	
CODE	DESCRIPTION
A	BLACK LEGEND AND BORDER ON A ORANGE BACKGROUND
B	BLACK LEGEND AND BORDER ON A WHITE BACKGROUND
C	WHITE LEGEND AND BORDER ON A GREEN BACKGROUND
D	WHITE LEGEND AND BORDER ON A RED BACKGROUND
E	RED LEGEND AND BORDER ON A WHITE BACKGROUND
F	BLACK LEGEND AND BORDER ON A FLORESCENT YELLOW GREEN BACKGROUND

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